

APPENDIX 1A:

GLENWOOD RIVERFRONT STREET CROSS-SECTIONS STANDARDS

SDC Table 4.2-1 of the Springfield Development Code provides the minimum street right-of-way width and the minimum curb-to-curb width for public streets in Springfield. During the adoption of the Glenwood Refinement Plan, the Springfield City Council approved street design policies and implementation strategies for use within the Glenwood Riverfront to promote the design and character of this area. The purpose of the Glenwood Riverfront street cross-sections standards is to assist in choosing the appropriate design standards for a particular street in the Glenwood Riverfront based on the approved Glenwood Refinement Plan. The cross-sections in this Appendix are various examples of each street type that include specific standards and an associated image.

The policy directions in the Glenwood Refinement Plan are intended to make all modes safer, more attractive, and more convenient, offering more options to all users for commuter travel, access to development, and recreational purposes. The objective is to create a highly pedestrian oriented environment to support future mixed-use development and increase the ease and convenience of walking. Ways to foster this pedestrian friendly environment are through decreasing automobile speeds in neighborhoods, focusing most through-traffic on arterials, aligning streets to reduce the distance that pedestrians have to walk to a crosswalk to safely cross a street, allowing sight lines and connections to destinations that attract pedestrian activity, and minimizing the real and perceived distances between development, parks, and greenway amenities. Policies also include features such as wide setback sidewalks with minimal interruptions in the flow or grade of pedestrian travel, interesting street furniture and public art, pedestrian-scale lighting, street trees, and other green street elements that also make the pedestrian experience safe, comfortable, and attractive.

Within the Glenwood Refinement Plan, the Glenwood Riverfront is subdivided into the Franklin Riverfront and McVay Riverfront. Although the street cross-section standards depicted in this Appendix are focused on the Franklin Riverfront, these standards also apply to the McVay Riverfront to the extent that a public street network is created in this area as development occurs.

Organization of Appendix 1A:

Appendix 1A is organized as follows:

SECTION 1A.1

- Section 1A.1a, “Riverfront Street with Parallel Parking”
“Riverfront Street with Parallel Parking Image”
- Section 1A.1b, “Riverfront Street with Angled Parking”
“Riverfront Street with Angled Parking Image”
- Section 1A.1c, “Riverfront Street with Reversed Angled Parking”
“Riverfront Street with Reversed Angled Parking Image”

SECTION 1A.2

- Section 1A.2a, “Park Block Street with Angled Parking on One Side of the Street”
“Park Block Street with Angled Parking on One Side of the Street Image”
- Section 1A.2b, “Park Block Street with Parallel Parking on Both Sides of the Street”
“Park Block Street with Parallel Parking on Both Sides of the Street Image”
- Section 1A.2c, “Park Block Street with Parallel Parking on One Side of the Street”
“Park Block Street with Parallel Parking on One Side of the Street Image”

SECTION 1A.3

- Section 1A.3a, “Collector Street with Buffered Bike Lane”
“Collector Street with Buffered Bike Lane Image”
- Section 1A.3b, “Collector Street with Separated Bike Lane”
“Collector Street with Separated Bike Lane Image”

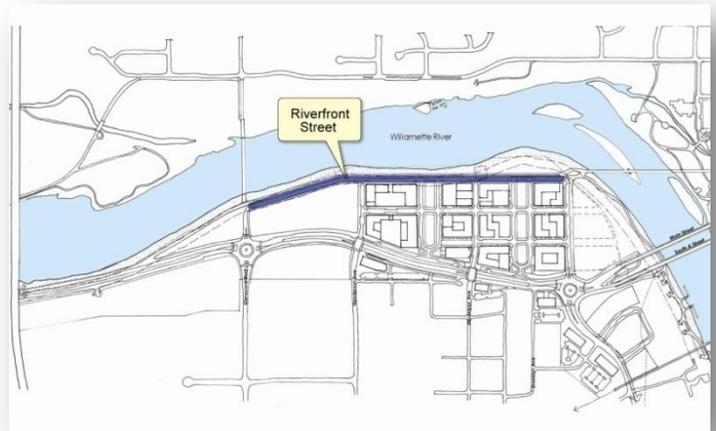
SECTION 1A.4

- Section 1A.4a, “Typical Street with Parallel Parking”
“Typical Street with Parallel Parking Image”

1A.1. Glenwood Riverfront Street

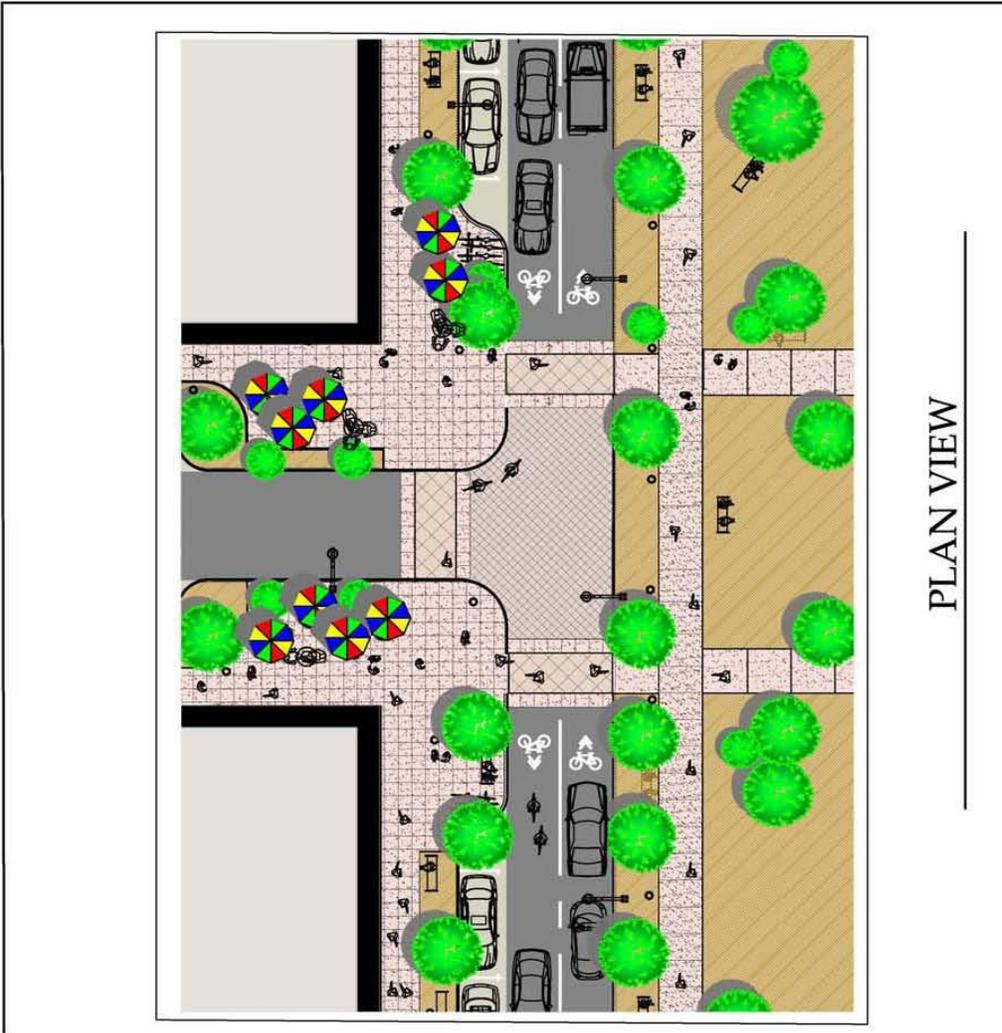
a. For the Glenwood Riverfront Street with parallel parking, the following Street standards shall be met:

1. The street shall be two way.
2. There shall be a minimum 7' setback sidewalk on each side of the street.
3. There shall be a minimum 7' amenity zone (elements in an amenity zone are street trees, plants, parking meters, street lights, street furniture, art, etc.) between the curb and sidewalk on the south side of the street.
4. There shall be a minimum 7' amenity zone between the travel way and sidewalk on the north side of the street.
5. There shall be decorative street lighting.
6. All intersections shall be raised with a distinctive stamped pattern and vibrant color.
7. Decorative bollards shall be used at the curb returns of the raised intersections.
8. All pedestrian crosswalks shall be raised crosswalks with a distinctive stamped pattern and vibrant color.
9. Parking bulb-outs shall extend 2' out from the parking striping.
10. Parallel parking stall widths shall be 7' wide.
11. Parking shall be installed on the south side of the street.
12. Parking shall not be installed on the north side of the street.
13. Travel lanes shall be 10' wide.





SECTION VIEW



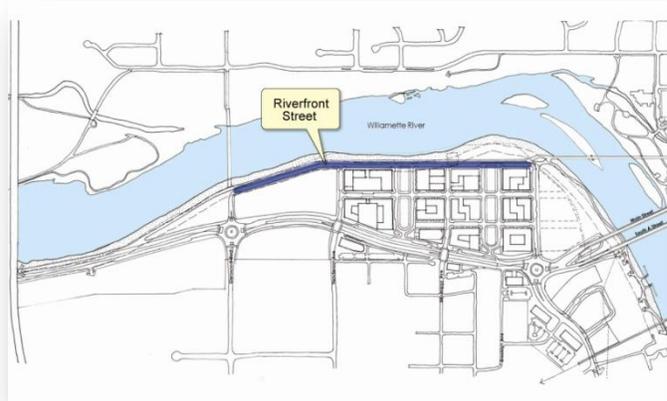
PLAN VIEW

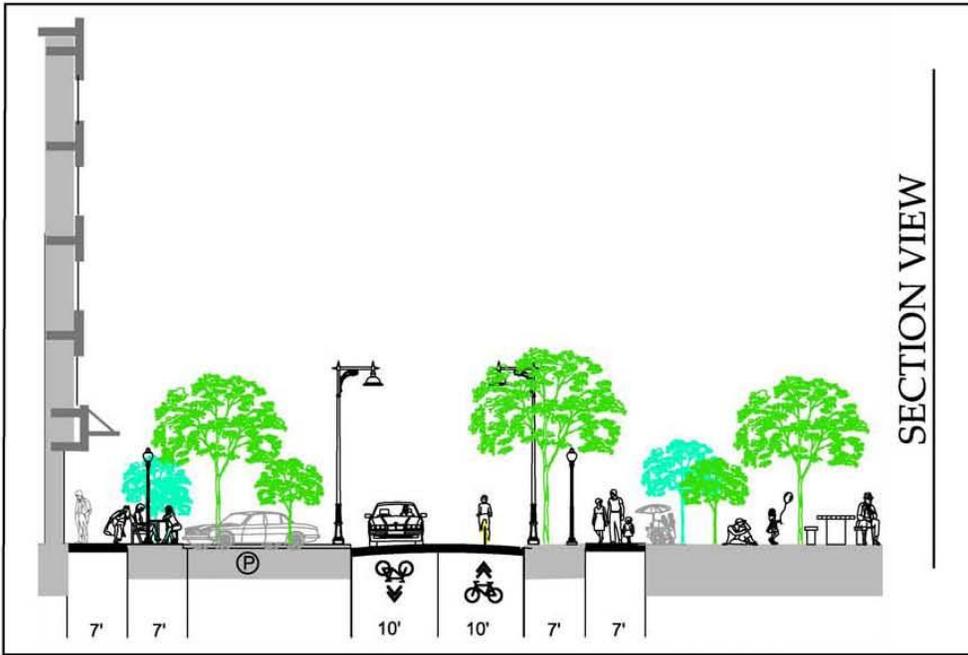
RIVERFRONT STREET - PARALLEL PARKING

1A. 1. Glenwood Riverfront Street

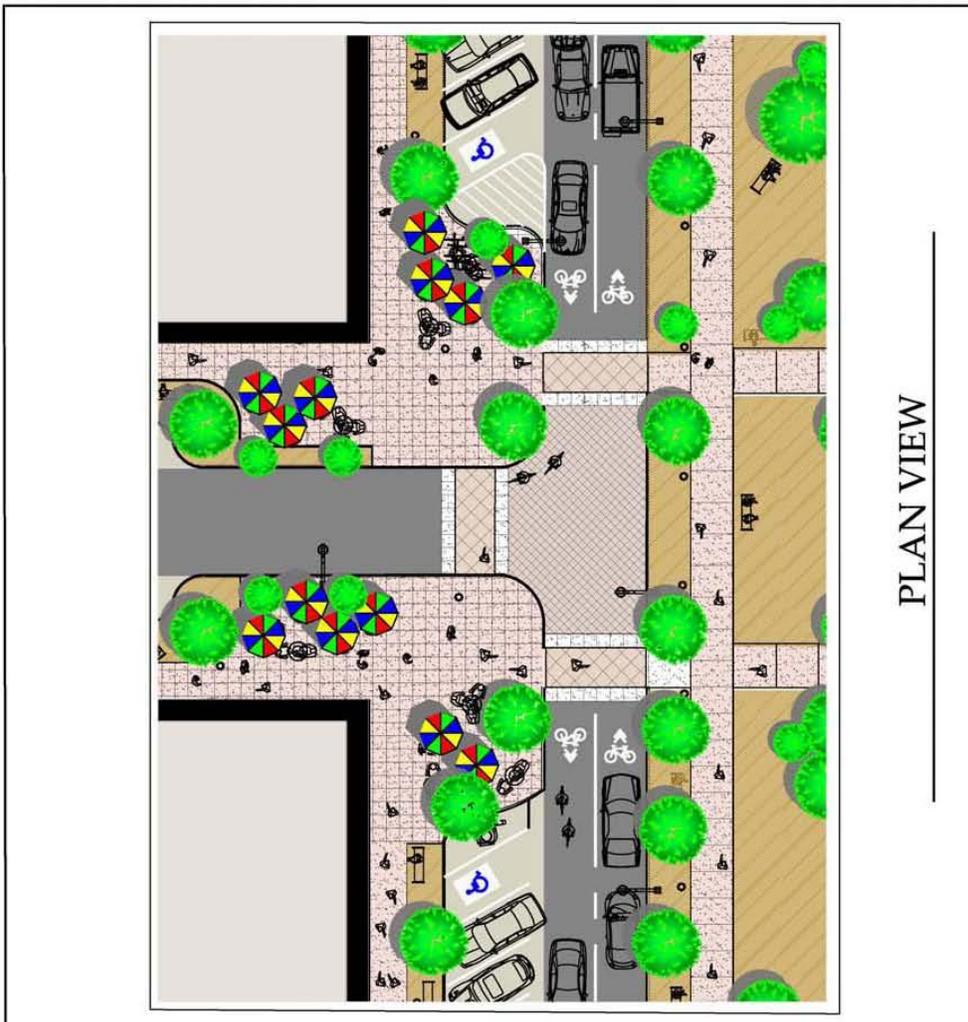
b. For the Glenwood Riverfront Street with angled parking, the following Street standards shall be met:

1. The street shall be two way.
2. There shall be a minimum 7' setback sidewalk on both sides of the street.
3. There shall be a minimum 7' amenity zone (elements in an amenity zone are street trees, plants, parking meters, street lights, street furniture, art, etc.) between the curb and sidewalk on the south side of the street.
4. There shall be a minimum 7' amenity zone between the travel way and sidewalk on the north side of the street.
5. There shall be decorative street lighting.
6. All intersections shall be raised with a distinctive stamped pattern and vibrant color.
7. Decorative bollards shall be used at the curb returns of the raised intersections.
8. All pedestrian crosswalks shall be raised crosswalks with a distinctive stamped pattern and vibrant color.
9. Parking bulb-outs shall extend 2' out from the angled parking striping.
10. The angled parking dimensions from the Springfield Development Code shall be used.
11. Parking shall be installed on the south side of the street.
12. Parking shall not be installed on the north side of the street.
13. Travel lanes shall be 10' wide.





SECTION VIEW



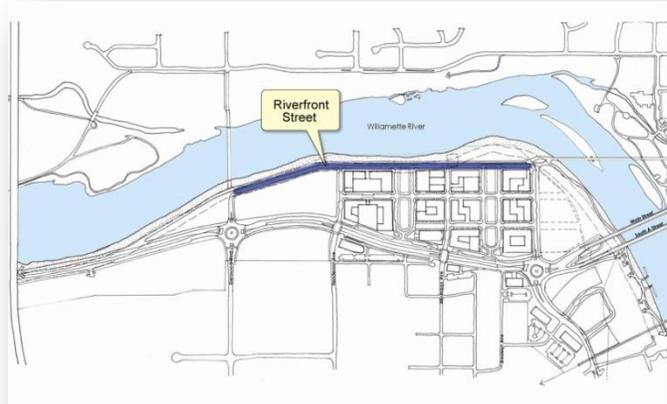
PLAN VIEW

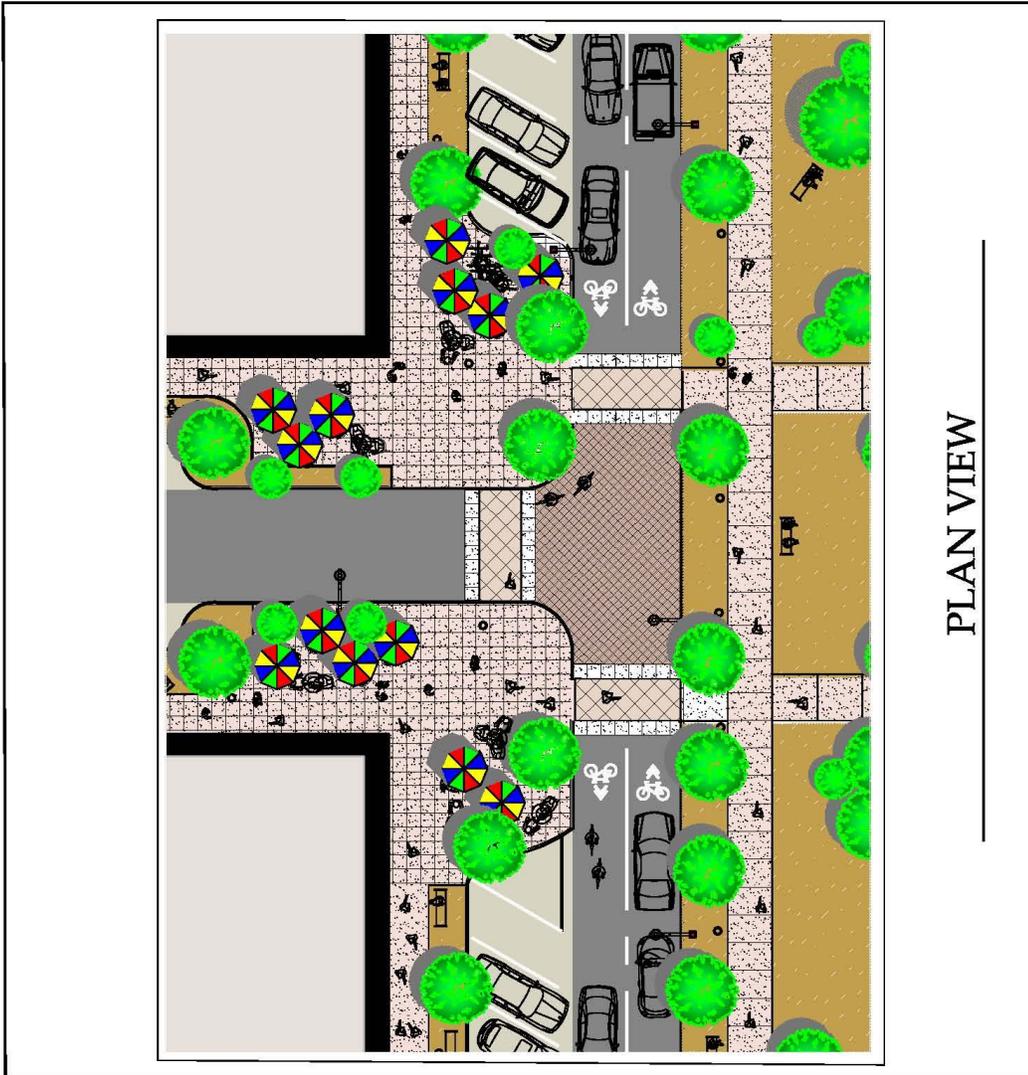
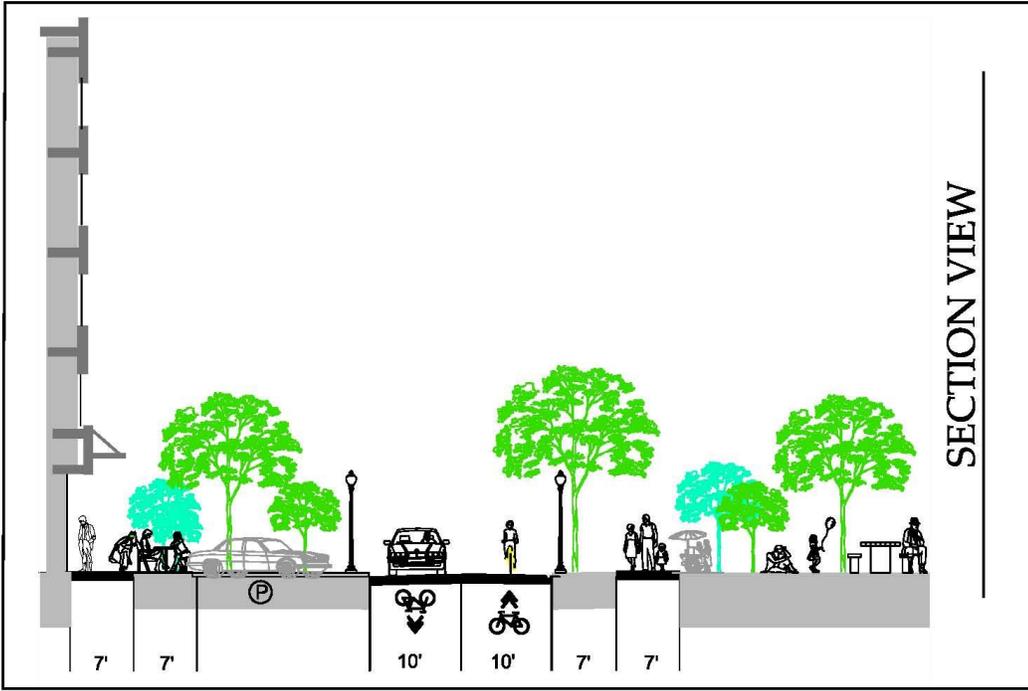
RIVERFRONT STREET - ANGLED PARKING

1A. 1. Glenwood Riverfront Street

c. For the Glenwood Riverfront Street with reverse angled parking, the following Street standards shall be met:

1. The street shall be two way.
2. There shall be a minimum 7' setback sidewalk on both sides of the street.
3. There shall be a minimum 7' amenity zone between the curb and sidewalk on the south side of the street.
4. There shall be a minimum 7' amenity zone between the travel way and sidewalk on the north side of the street.
5. There shall be decorative street lighting.
6. All intersections shall be raised with a distinctive stamped pattern and vibrant color.
7. Decorative bollards shall be used at the curb returns of the raised intersections.
8. All pedestrian crosswalks shall be raised crosswalks with a distinctive stamped pattern and vibrant color.
9. Parking bulb-outs shall extend 2' out from the reverse angled parking striping.
10. The reverse angled parking dimensions from the Springfield Development Code shall be used.
11. Parking shall be installed on the south side of the street.
12. Parking shall not be installed on the north side of the street.
13. Travel lanes shall be 10' wide.





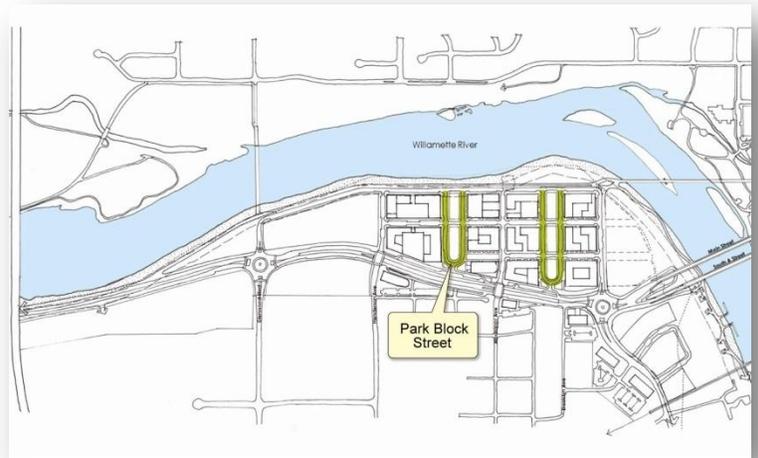
RIVERFRONT STREET - REVERSE ANGLED PARKING

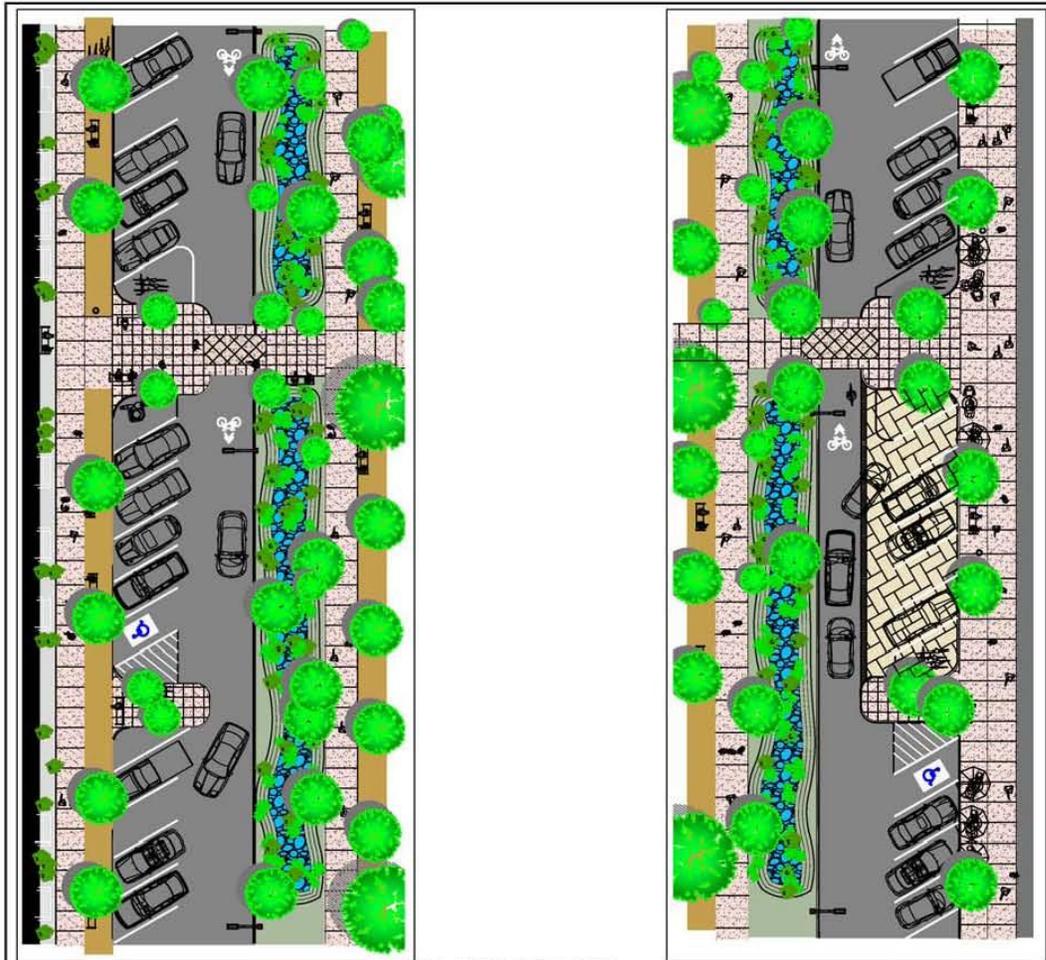
**1A.2. Glenwood Riverfront Area North/South
Park Block Streets**

a. For the Glenwood Riverfront Area Park Block Streets with angled parking on one side, the following Street standards shall be met:

1. The street shall be one way.
2. There shall be a minimum 7' setback sidewalk adjacent to the development.
3. There shall be a minimum 7' amenity zone on the Development side of the street between the curb and sidewalk. Appurtenances may include trees in tree wells, bicycle parking, tables, benches, street lights, landscaping, public art, etc.
4. There shall be decorative street lighting.
5. All intersections shall be raised with a distinctive stamped pattern and vibrant color.
6. Decorative bollards shall be used at the curb returns of the raised intersections.
7. All pedestrian crosswalks shall be raised crosswalks with a distinctive stamped pattern and vibrant color.
8. The park block from curb to curb shall be a minimum of 150'.
9. The angled parking dimensions from the Springfield Development Code shall be used.
10. Parking bulb-outs shall extend 2' out from the angled parking striping.
11. Angled parking shall only be on one side of the street.
12. Storm water treatment shall be installed between the curb and sidewalk. It shall treat all impervious surfaces that are in the public right of way and shall be sized accordingly.

13. There shall be a minimum 8' sidewalk between the stormwater treatment and park block.
14. The travel lane shall be 10' wide.





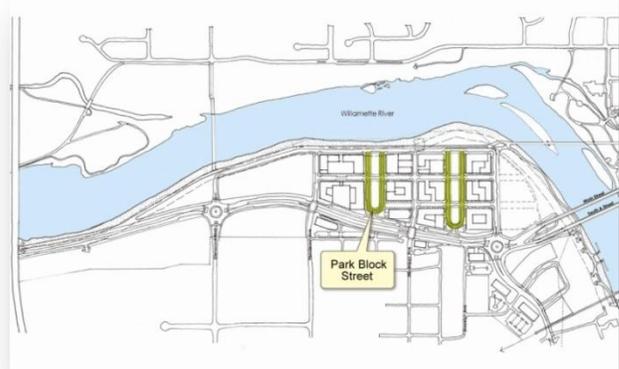
PARK BLOCK STREET - ANGLED PARKING ONE SIDE

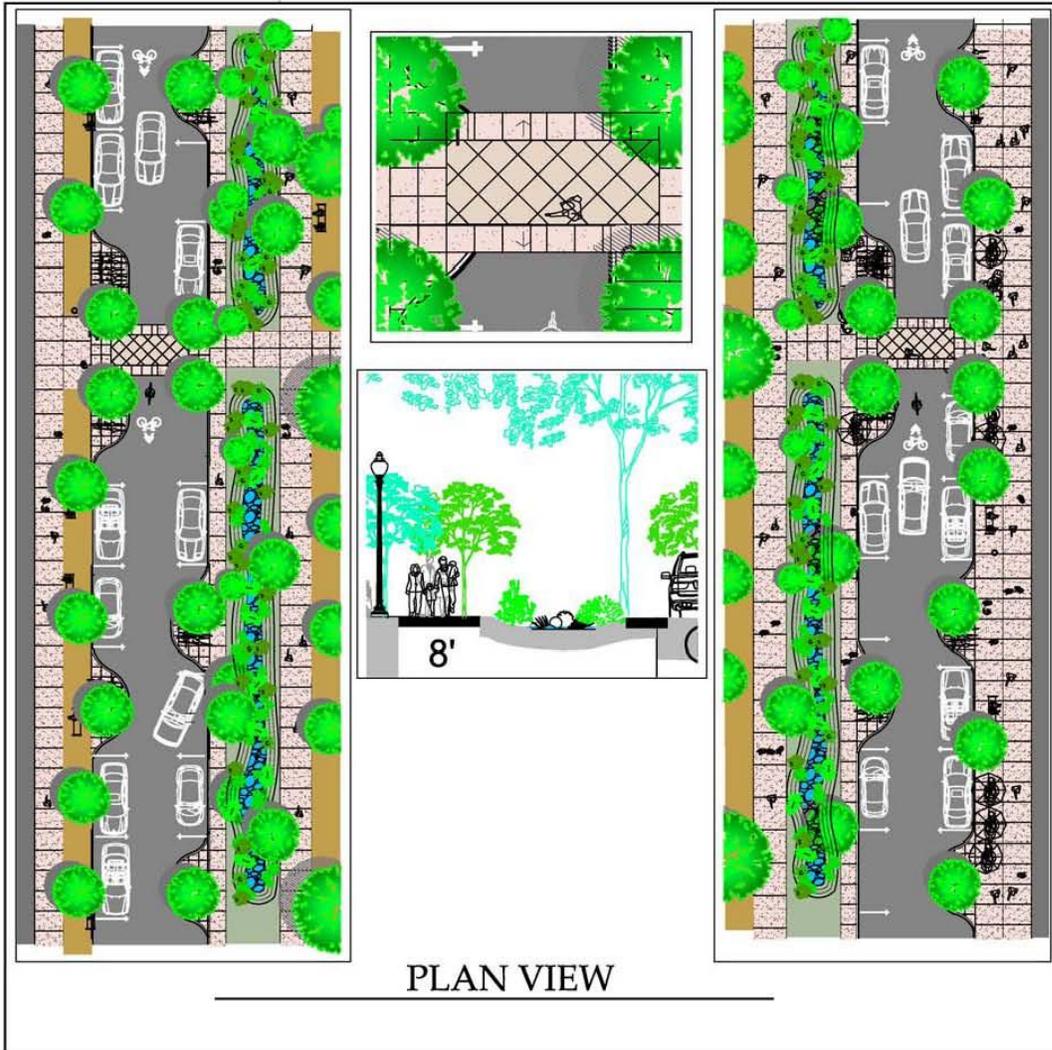
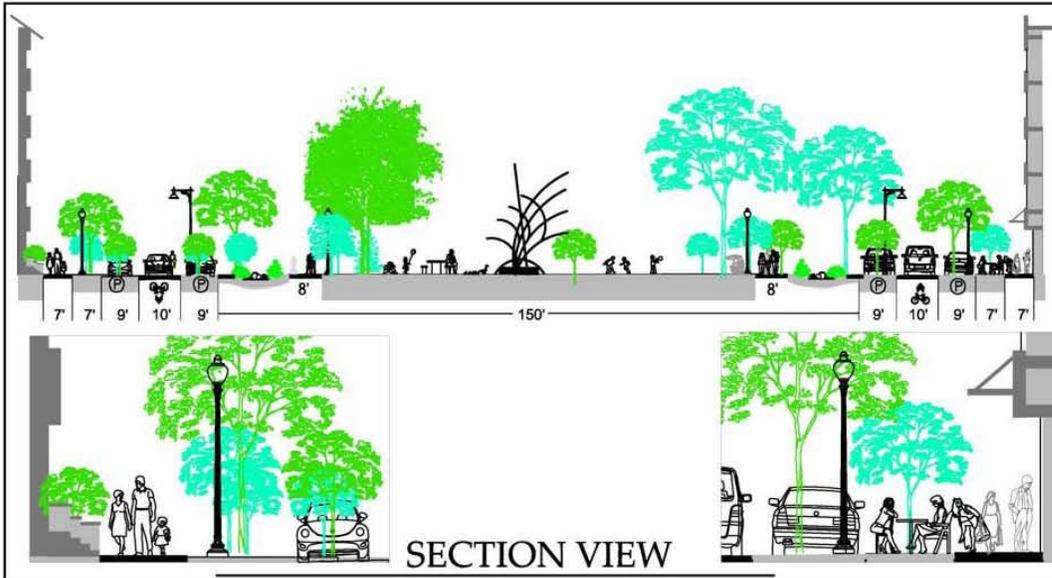
**1A. 2. Glenwood Riverfront Area North/South
Park Block Streets**

b. For the Glenwood Riverfront Area Park Block Streets with parallel parking on both sides, the following Street standards shall be met:

1. The street shall be one way.
2. There shall be a minimum 7' setback sidewalk adjacent to the development.
3. There shall be a minimum 7' amenity zone on the Development side of the street between the curb and sidewalk. Appurtenances may include trees in tree wells, bicycle parking, tables, benches, street lights, landscaping, public art, etc.
4. There shall be decorative street lighting.
5. All intersections shall be raised with a distinctive stamped pattern and vibrant color.
6. Decorative bollards shall be used at the curb returns of the raised intersections.
7. All pedestrian crosswalks shall be raised crosswalks with a distinctive stamped pattern and vibrant color.
8. The park block from curb to curb shall be a minimum of 150'.
9. Parking bulb-outs shall extend 2' out from the parking striping.
10. Bulb-outs should be staggered to create the narrow travel way.
11. Parallel parking stall widths shall be 7' wide.
12. Storm water treatment shall be installed between the curb and adjacent sidewalk. It shall treat all impervious surfaces that are in the public right of way and shall be sized accordingly.
13. There shall be a minimum 8' sidewalk between the storm water treatment and park block.
14. The travel lane shall be 10' wide.

15. A 2' paved pedestrian travel way shall be between the parking lane and the storm water treatment.





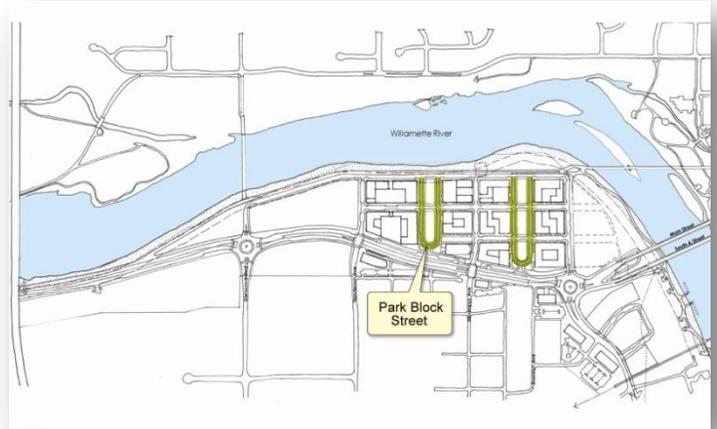
PARK BLOCK STREET - PARALLEL PARKING TWO SIDES

**1A.2. Glenwood Riverfront Area North/South
Park Block Streets**

c. For the Glenwood Riverfront Area Park Block Streets with parallel parking on one side, the following Street standards shall be met:

1. The street shall be one way.
2. There shall be a minimum 7' setback sidewalk adjacent to the development.
3. There shall be a minimum 7' amenity zone on the Development side of the street between the curb and sidewalk. Appurtenances may include trees in tree wells, bicycle parking, tables, benches, street lights, landscaping, public art, etc.
4. There shall be decorative street lighting.
5. All intersections shall be raised with a distinctive stamped pattern and vibrant color.
6. Decorative bollards shall be used at the curb returns of the raised intersections.
7. All pedestrian crosswalks shall be raised crosswalks with a distinctive stamped pattern and vibrant color.
8. The park block from curb to curb shall be a minimum of 150'.
9. Parking bulb-outs shall extend 2' from the parking striping.
10. Parallel parking stall widths shall be 7' wide.
11. Storm water treatment shall be installed between the curb and sidewalk. It shall treat all impervious surfaces that are in the public right of way and shall be sized accordingly.
12. There shall be a minimum 2' paved walking width between the parking and storm water treatment area.

13. There shall be a minimum 8' sidewalk between the storm water treatment and park block.
14. The travel lane shall be 10' wide.



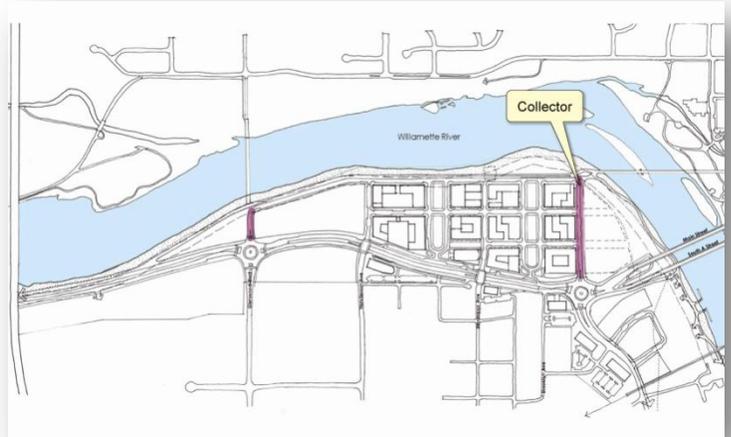


PARK BLOCK STREET - PARALLEL PARKING ONE SIDE

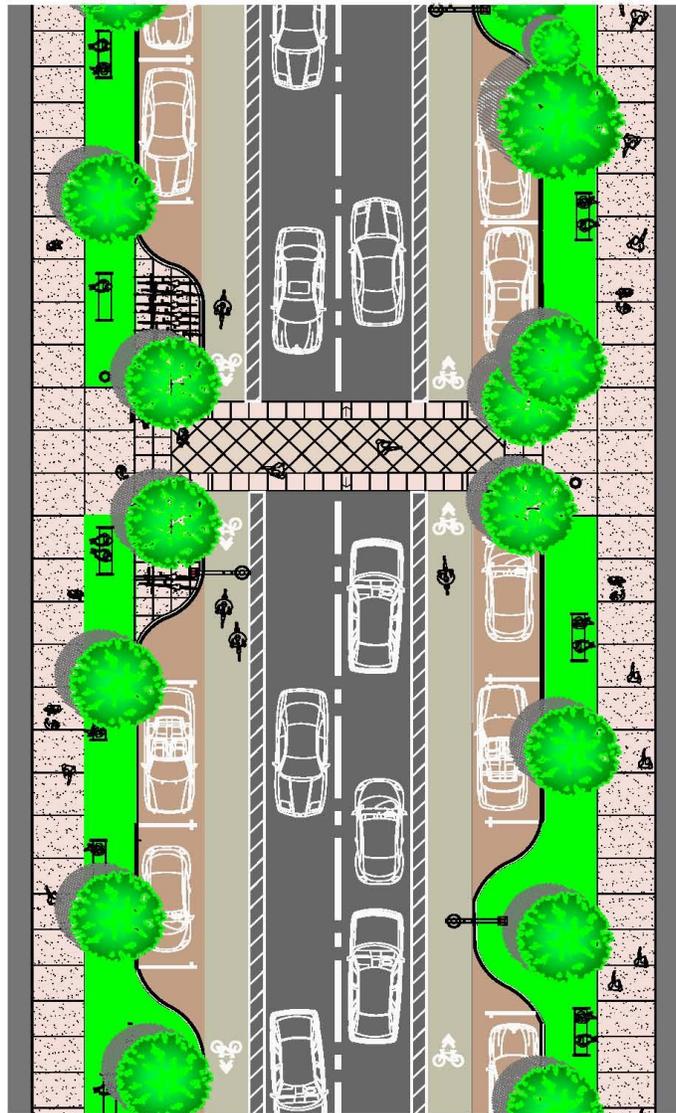
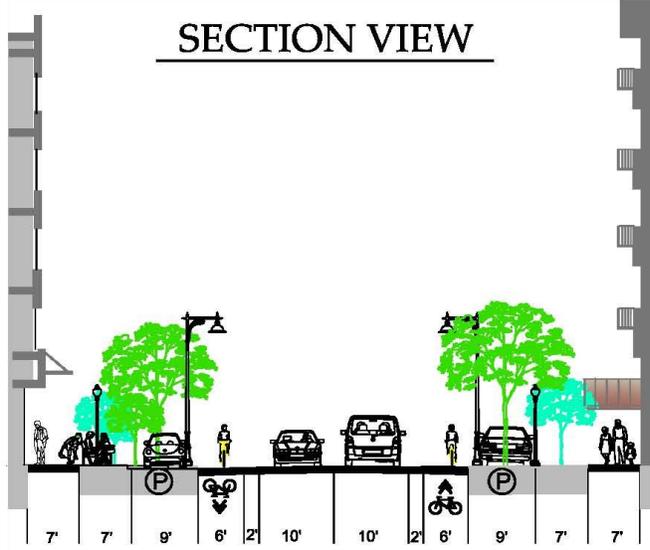
1A.3. Glenwood Riverfront Area Collector Streets

a. For the Glenwood Riverfront Area Collector Streets with buffered bicycle lane, the following Street standards shall be met:

1. The street shall be two way.
2. There shall be a minimum 7' setback sidewalk adjacent to the development.
3. There shall be a minimum 7' amenity zone between the curb and sidewalk. Appurtenances may include trees in tree wells, bicycle parking, tables, benches, street lights, landscaping, public art, etc.
4. There shall be decorative street lighting.
5. All intersections shall be raised with a distinctive stamped pattern and vibrant color.
6. Decorative bollards shall be used at the curb returns of the raised intersections.
7. All pedestrian crosswalks shall be raised crosswalks with a distinctive stamped pattern and vibrant color.
8. Parking bulb-outs shall be a minimum of 9' wide.
9. Parallel parking stall widths shall be 7' wide.
10. Travel lane shall be 10' wide.
11. Bicycle lanes shall be 6' wide with a striped 2' buffered area between the bicycle lane and the travel lane and a striped 2' buffered area between the bicycle lane and parking lane



SECTION VIEW



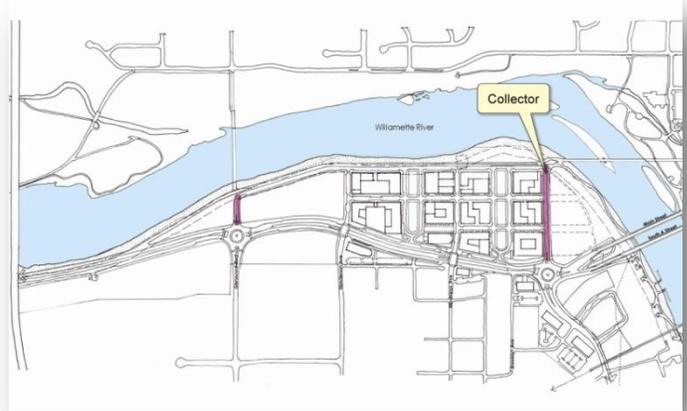
PLAN VIEW

COLLECTOR - BUFFERED BIKE LANE

1A.3. Glenwood Riverfront Area Collector Streets

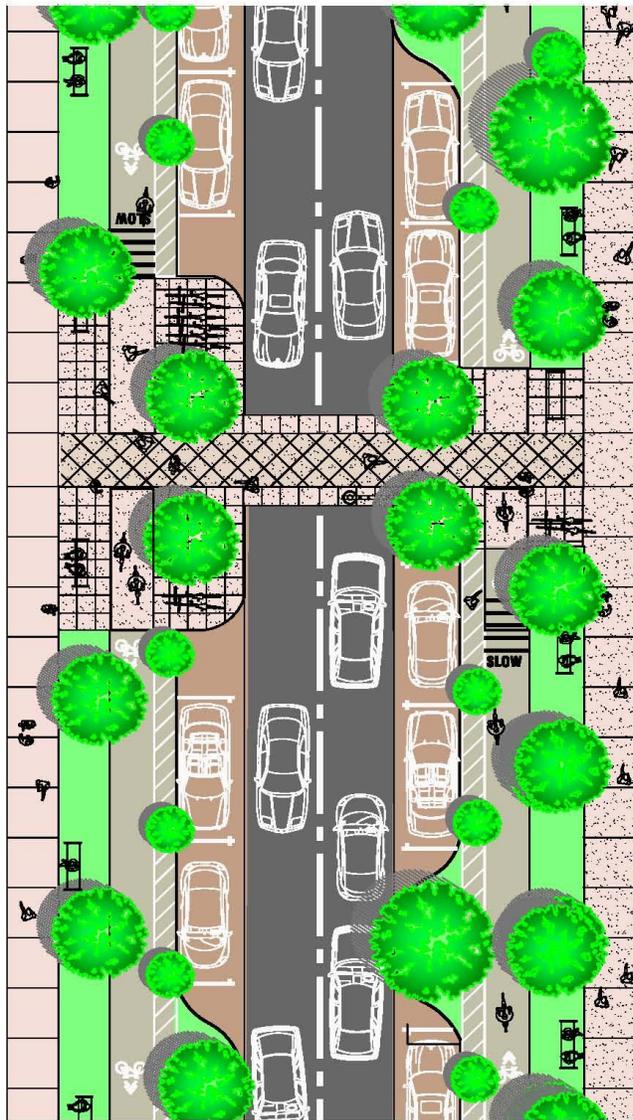
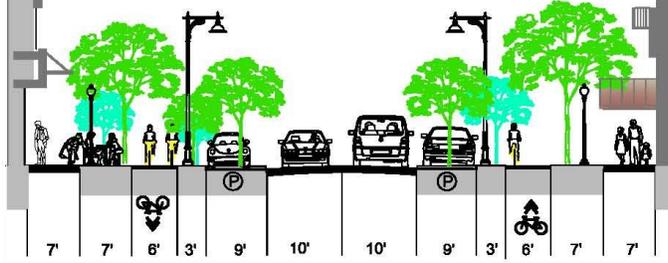
b. For the Glenwood Riverfront Area Collector Streets with separated bicycle lane, the following Street standards shall be met:

1. The street shall be two way.
2. There shall be a minimum 7' setback sidewalk adjacent to the development.
3. There shall be a minimum 7' amenity zone between the curb and sidewalk. Appurtenances may include trees in tree wells, bicycle parking, tables, benches, street lights, landscaping, public art, etc.
4. There shall be decorative street lighting.
5. All intersections shall be raised with a distinctive stamped pattern and vibrant color.
6. Decorative bollards shall be used at the curb returns of the raised intersections.
7. All pedestrian crosswalks shall be raised crosswalks with a distinctive stamped pattern and vibrant color.
8. Parking bulb-outs shall extend 2' out from the parking striping.
9. Parallel parking stall widths shall be 7' wide.
10. Travel lane shall be 10' wide.
11. Bicycle lanes shall be 6' wide with a 3' buffered area between the bicycle lane and the parking lane and elevated to the same elevation as the sidewalk to not be at the same grade as the parking lane.



(Rendering: www.bikelongbeach.org)

SECTION VIEW



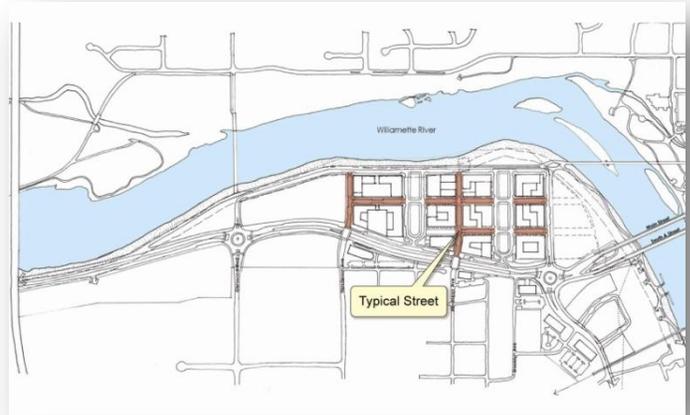
PLAN VIEW

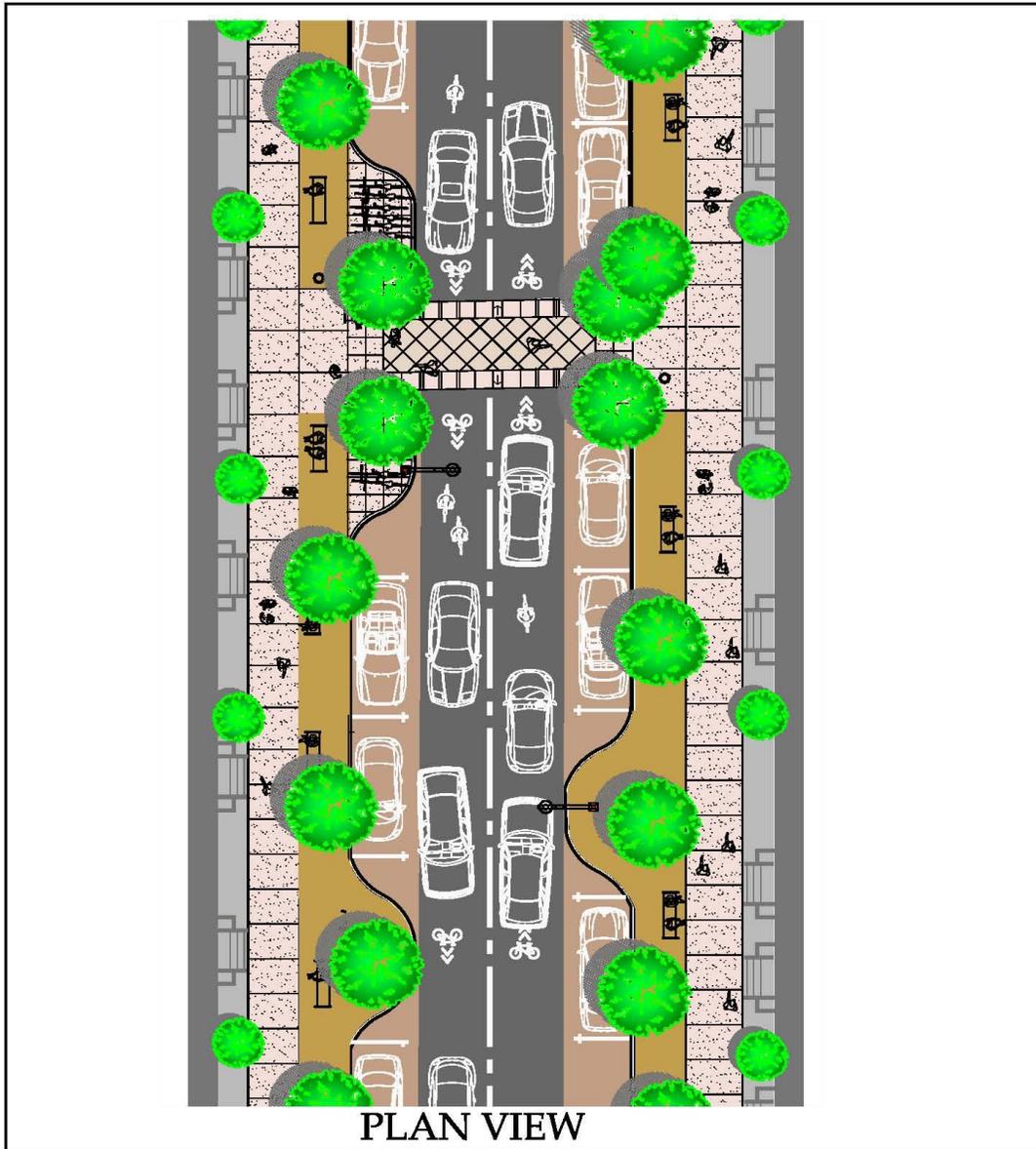
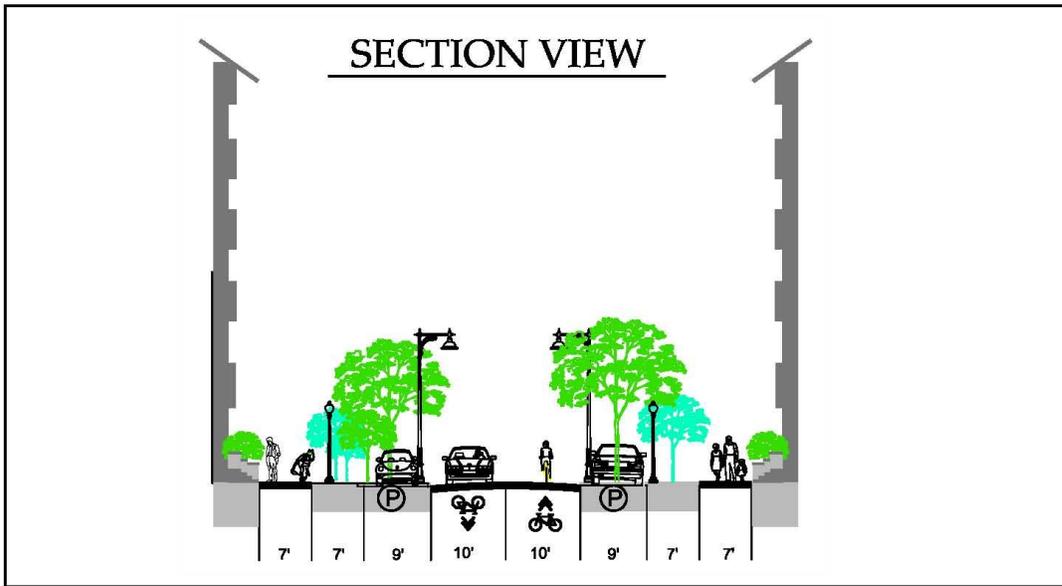
COLLECTOR - SEPARATED BIKE LANE

1A.4. Glenwood Riverfront Area Typical Streets

For the Glenwood Riverfront Area Typical Streets, the following Street standards shall be met:

1. The street shall be two way.
2. There shall be a minimum 7' setback sidewalk on each side of the street.
3. There shall be a minimum 7' amenity zone between the curb and sidewalk. Appurtenances may include trees in tree wells, bicycle parking, tables, benches, street lights, landscaping, public art, etc.
4. There shall be decorative street lighting.
5. All intersections shall be raised with a distinctive stamped pattern and vibrant color.
6. Decorative bollards shall be used at the curb returns of the raised intersections.
 1. All pedestrian crosswalks shall be raised crosswalks with a distinctive stamped pattern and vibrant color.
 2. Parking bulb-outs shall extend 2' out from the parking striping.
 3. Parallel parking stall widths shall be 7' wide.
 4. Travel lanes shall be 10' wide.





TYPICAL - PARALLEL PARKING